

APPROVED

17 Avenue SW

urban design strategy







APPROVED

17 Avenue SW

urban design strategy

PUBLISHING INFORMATION

TITLE: 17 AVENUE SW URBAN DESIGN STRATEGY

AUTHOR: LAND USE PLANNING & POLICY
PLANNING, DEVELOPMENT & ASSESSMENT

STATUS: APPROVED 2008 MARCH 10

PRINTING DATE: 2008 MARCH

ADDITIONAL COPIES: THE CITY OF CALGARY
RECORDS & INFORMATION MANAGEMENT (RIM)
P.O. BOX 2100, STN "M", #8115
CALGARY, ALBERTA T2P 2M5

PHONE: (403) 268-5333 or Call 3-1-1

FAX: (403) 268-4615

Table of Contents

Acknowledgment.....	ii	7.0 Case Studies	37
1.0 Background.....	1	7.1 East Precinct Context:	38
2.0 Role of <i>The Strategy</i>	2	Case Study 1: Stampede Entrance	38
3.0 Stakeholder Engagement	3	Case Study 2: Rouleauville Park	40
4.0 Local Character Discussion	4	7.2 Central Precinct.....	42
4.1. The East Precinct	6	Case Study 3: Tomkins Park.....	42
4.2 The Central Precinct.....	8	7.3 West Precinct	44
4.3 The West Precinct.....	10	Case Study 4: 11 Street S.W. Intersection	44
5.0 Uses.....	12	8.0 Implementation.....	46
5.1 Uses & Use Sizes	12		
6.0 Public Realm Guidelines	13		
6.1 Pedestrian Realm Guidelines	14		
6.1.1 “Build to” Incentive	15		
6.1.2 Pedestrian Realm Scenarios.....	15		
6.2 Built Form Guidelines.....	30		



Acknowledgment

The City wishes to acknowledge the contributors of Sturgess Architecture in the stakeholder engagement and analysis components of this Strategy. Section 7.0 Case Studies is the product of Sturgess Architecture.

1.0 Background

Once a boundary to the city in the early 1900's, 17 Avenue SW has always been an important artery in Calgary. It marks the intersection of the CPR established downtown grid with the alignment of the greater city grid. It is the original surveyor's correction line with the confluence of the two grids occurring at the centre of the avenue at Tomkins Park.

Today, 17 Avenue SW is often viewed by many Calgarians as one of the City's most vital and thriving thoroughfares. The avenue is a medley of mixed-use commercial, residential and office in varying building forms providing fluctuating degrees and quality of public realm. As mentioned in the Beltline Area Redevelopment Plan (ARP), public realm can be defined as "special places, linkages, interfaces and the building form which are fundamental to the creation of a functional, visually attractive and safe public realm."

The study area is comprised of both the north and south sides of a sixteen block stretch between MacLeod Trail SE to 14 Street SW. There currently exists a need within this portion of 17 Avenue SW to define and identify the character of the avenue so that it can be preserved and enhanced through future development.

This avenue has been identified by the *Centre City Plan* as a *High Street*, typically found in areas with historic character or high-density retail areas with a high concentration of pedestrian movement. High streets represent a model of the traditional main street with mixed use, medium to high-density developments, and provide flexible transportation alternatives

including on-street parking, appropriate plantings and an animated and safe pedestrian environment. The pedestrian realm, especially the interface zone must accommodate a full range of activities from retail to patios. High streets are characterized by:

- Upgraded sidewalks;
- Public art;
- Pedestrian scaled street lighting;
- Single tree boulevards;
- Pedestrian scaled built form; and,
- Enhanced street furniture.

The Strategy provides urban design guidelines for this portion of 17 Avenue SW. The guidelines aim to preserve the unique personalities of different sections of the avenue, while providing a cohesive and consistent guideline for future development along the whole avenue. The guidelines will address:

- Pedestrian oriented development;
- Building massing and scale; and
- Interfaces between public and private realm.

2.0 Role of *The Strategy*

The 17 Avenue SW Urban Design Strategy is a non-statutory document that provides guidelines for development and makes recommendations for future action. It addresses the interaction and relationship between buildings, landscape, road right of ways and public spaces to encourage livability and vitality along the avenue. There are four communities which are represented along the thoroughfare.

These communities are:

- *Mission*: 1 St. SE to 4 St. SW on the south side of 17 Avenue;
- *Cliff Bungalow*: 4 St. SW and College Lane SW on the south side of 17 Avenue SW;
- *Lower Mount Royal*: College Lane SW and 14 St SW on the south side of 17 Avenue SW; and,
- *Beltline*: Macleod Trail SE and 1 St SE on the south side of 17 Avenue SW and between Macleod Trail SE and 14 St SW on the north side of 17 Avenue SW.

Each of the communities has an Area Redevelopment Plan (ARP) that guides land use and development for the area. Applicable ARP's include:

- *Mission ARP*;
- *Cliff Bungalow ARP*;

- *Lower Mount Royal ARP*; and,
- *The Beltline ARP*.

The Strategy will be used in addition to these documents to guide design for new development. Other applicable plans include:

- *The Centre City Plan*

This plan is non-statutory in nature, but does contain ideas, actions and projects that relate to the avenue.

Currently there is no unified strategy for the 17 Avenue SW corridor to address urban design ideas and guidelines for sidewalks, streetscapes, public spaces, and built form along both sides of the avenue. *The Strategy* is sensitive to the character of the avenue and surrounding neighbourhoods. It strives to maintain a high quality of urban design and built form similar to the character of the avenue that Calgarians have come to expect.

3.0 Stakeholder Engagement

Comprehensive consultation with stakeholders ensured that public opinion and values were taken into account in the creation of these guidelines. Stakeholders from the above mentioned communities, as well as local Business Revitalization Zones were consulted through two public open houses held on September 20th 2005, and October 25th, 2005. The intent of these open houses was to gather information on issues relating to developments along the avenue. These open houses identified a number of design aspects such as:

- Building typology;
- Building character;
- Urban edges;
- Building interface
- Building massing; and,
- The quality of the public realm.

A stakeholder presentation was held to review the ideas prior to the final open house held at the beginning of January, 2006.

Between 2005 and 2007, significant work was completed on the *Centre City Plan* which identified street and road typologies vital to the development of 17 Avenue SW. Work on this plan also included significant stakeholder engagement and discussed the vision for 17 Avenue SW along with the rest of the Centre City. Additionally, new ARP's were approved for Mission and Beltline, which have further clarified the future

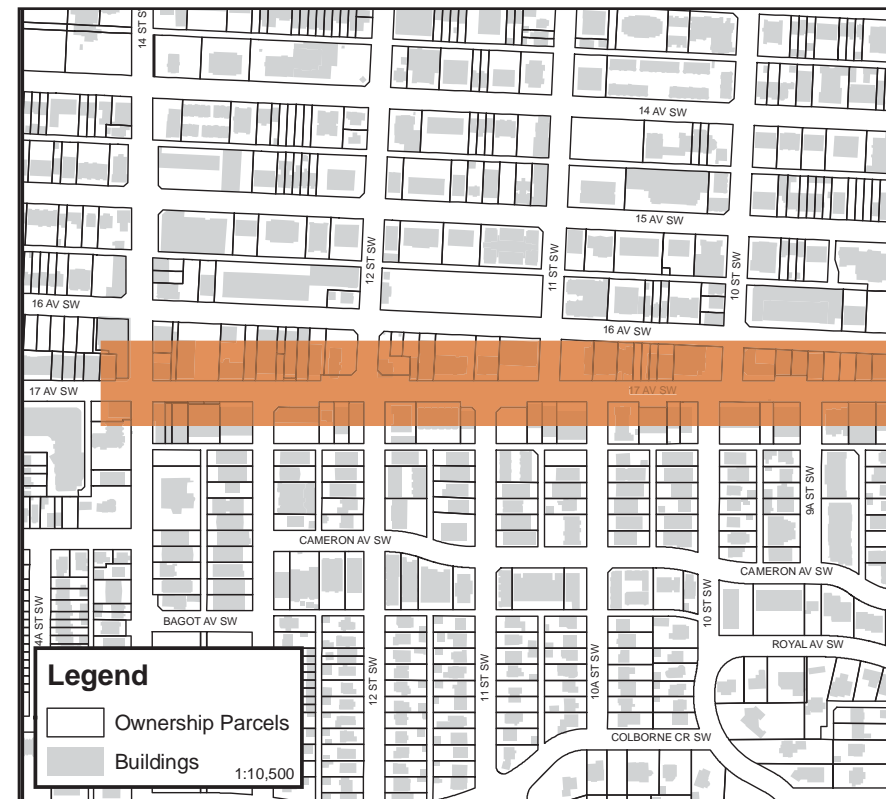
development expected within the area in terms of use, density and design. Each of these ARP's were informed by the 17 Avenue SW engagement input. Also, the new *Land Use Bylaw 1P2007* has created new districts for commercial corridors that apply to 17 Avenue SW. These new districts have incorporated provisions that address some of the concerns raised through the earlier 2005 engagement process. With these pieces now in place, *The Strategy* brings them together in a unified vision for the 17 Avenue SW corridor.

4.0 Local Character Discussion

The local context of an area includes a variety of physical aspects which are visible and tangible; such as elements pertaining to the built form, redevelopment opportunities, and open space patterns. The Beltline ARP defines character areas as: “areas that are identifiable as having special or unique qualities that are different from neighbouring areas within the community”. Using this thinking, the avenue can be differentiated into three different precincts, each with its own unique character, personality and potential.

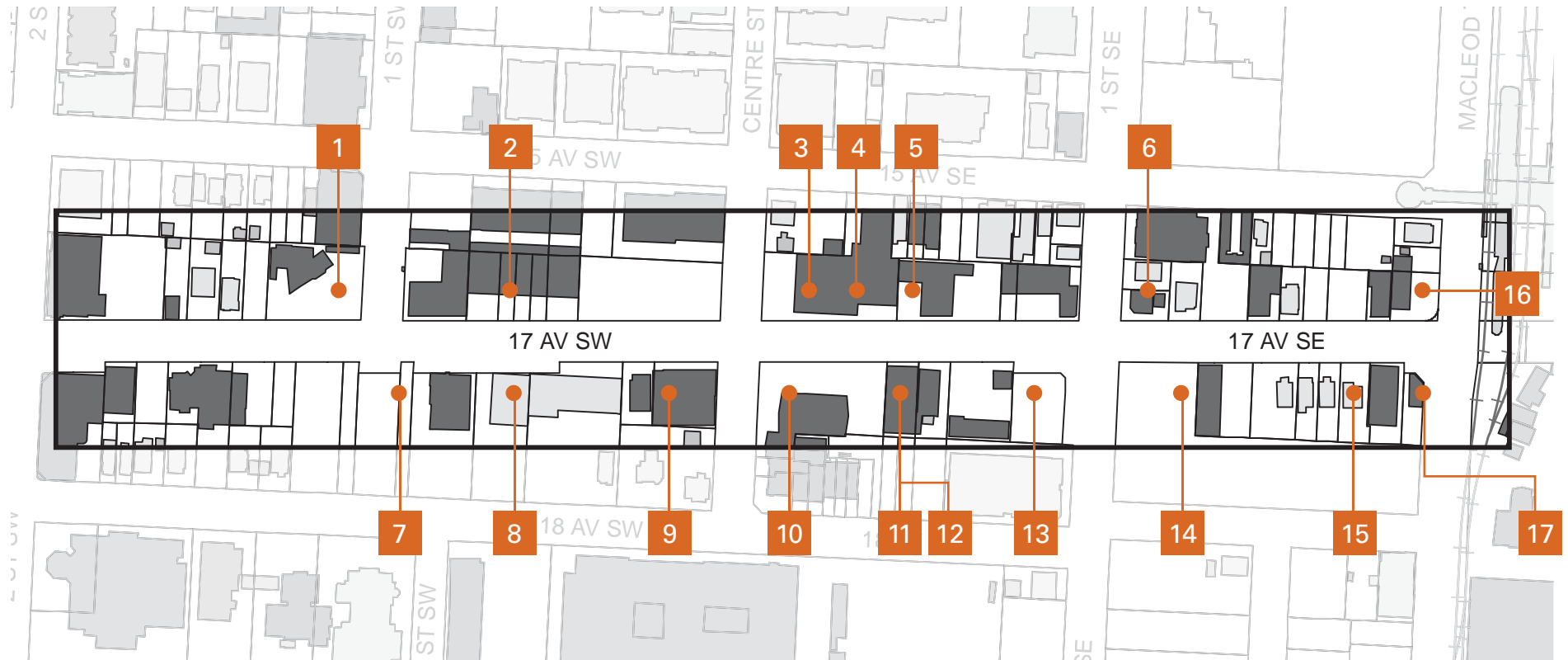
The first precinct is located between MacLeod Trail SE and 2 Street SW (the East Precinct), the second precinct from 2 Street SW to 9th Street SW (The Central Precinct), and the third precinct between 9th Street SW and 14 Street SW (the West Precinct).

MAP 1: Study Area





4.1. The East Precinct





Built Form

3. Inconsistent front setbacks & street wall
4. Single purpose buildings (mainly auto related)
5. Low quality architecture
6. Variety of building forms ranging mainly from one to two storey buildings
9. Encourages automotive traffic
14. Very little transparency at the pedestrian level

Building Interface

1. Minimal interaction with the space
2. Overhead power-lines dominating sidewalk
8. Large signage encroaching into the public realm for each individual use
10. Front parking lots
11. Automotive specialty and repair dominate the node in terms of use
12. Uneven pavement & deteriorating sidewalk quality

13. Many “dead” spaces

17. Varying sidewalk width

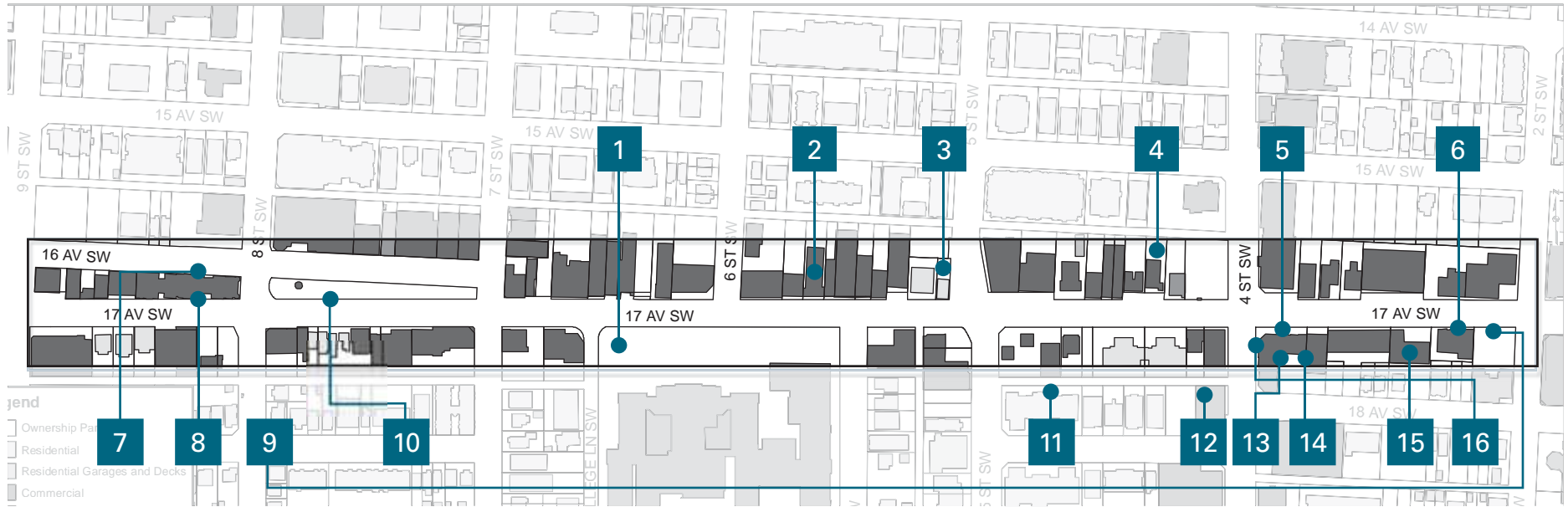
Linkages

16. Bound by Light Rail Transit (LRT) lines & a pedestrian ramp
16. No ground level access points for either pedestrians or vehicles through to Stampede Park

Special Places

7. Rouleauville Park/St Mary's
15. Heritage Conversions

4.2 The Central Precinct





Built Form

2. Mainly retail and restaurants with some intermittent office use.
2. Pedestrian oriented
2. Building height ranging mainly from 1-3 storeys.
2. Presence of an urban edge scaled for human use rather than vehicular use.
4. Patio setbacks range from 1.5–3.0 metres from the back of curb.
5. Setbacks ranging from 3.0-6.0 metres from the back of curb to the building front.
6. At-grade, individually accessed uses
11. Some buildings/sites are not currently pedestrian oriented

Building Interface

5. More continuous building frontage
7. Narrow pedestrian right of ways at specific spots
9. Improved lighting standards,
9. Banners for signage
12. Improved pavement materials

13. On-street parking begins at 2 St SW,
14. Avenue becomes more textured
15. Some parking at the front of buildings at east end of precinct

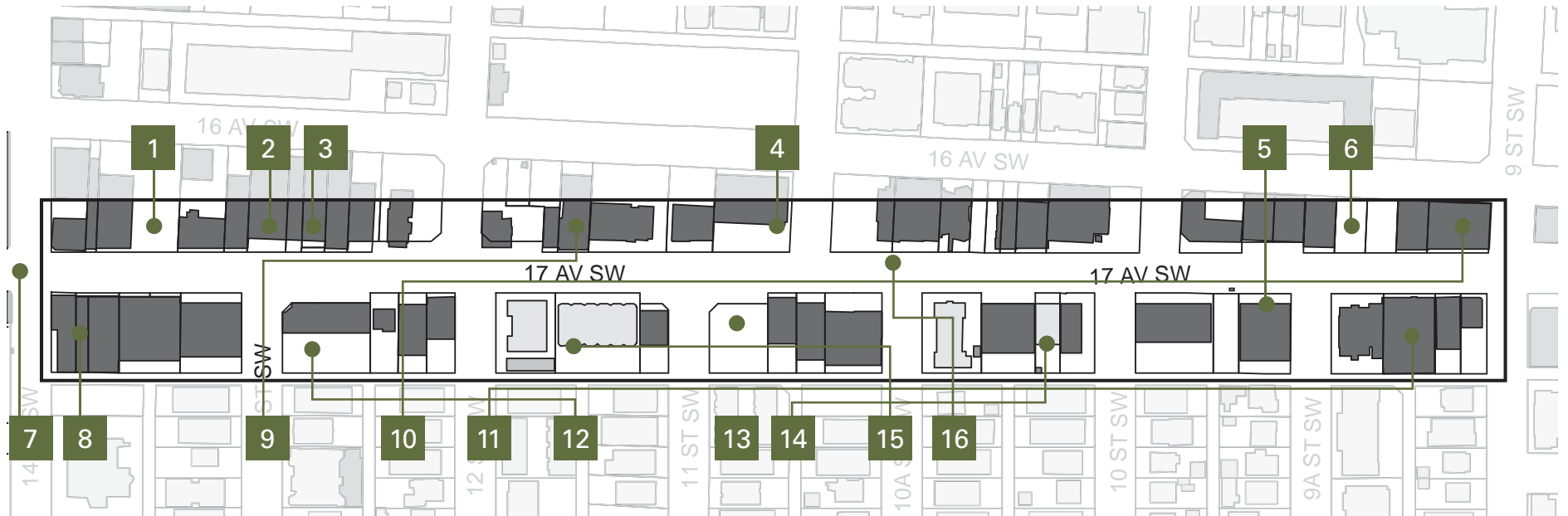
Linkages

1. Connections to 4 St SW & 8th St SW

Special Places

1. Western Canada High School
3. Heritage Building
8. The Devenish building
10. Tomkins Park
16. Public Art

4.3 The West Precinct





Built Form

1. Some lot access driveways
2. Trees spaced every 10.0- 20.0 metres
3. Inconsistent building frontage
6. Surface parking lots
6. Various forms of street furniture located between back of curb and building front
9. Building setbacks ranging from 3.0-10.0 metres
13. Some vacant sites
15. Higher densities for residential
16. Intermittent historic lamp posts

Building Interface

2. Big display windows – transparent to pedestrian
4. Large, auto-oriented signage
5. Larger storefronts
9. Automotive oriented spaces
10. New retail developments

11. Pedestrian oriented spaces

14. Most redevelopment consistent with older setbacks

Linkages

7. 14 Street SW, 11th St SW

Special Places

8. Heritage building

5.0 Uses

General

One of the main character defining elements of 17 Avenue SW is the variety of uses and the spacing between uses at the street level. The new C-COR-1 District of Land Use Bylaw 1P2007 ensures that street level uses remain small in size (a maximum of 465 square metres) and that they are predominantly retail and service oriented. For example, offices and medical clinics are not allowed on the ground floor and large floor area uses are to be located above or below street level. These rules are entirely consistent with the overall vision for 17 Avenue SW.

5.1 Uses & Use Sizes

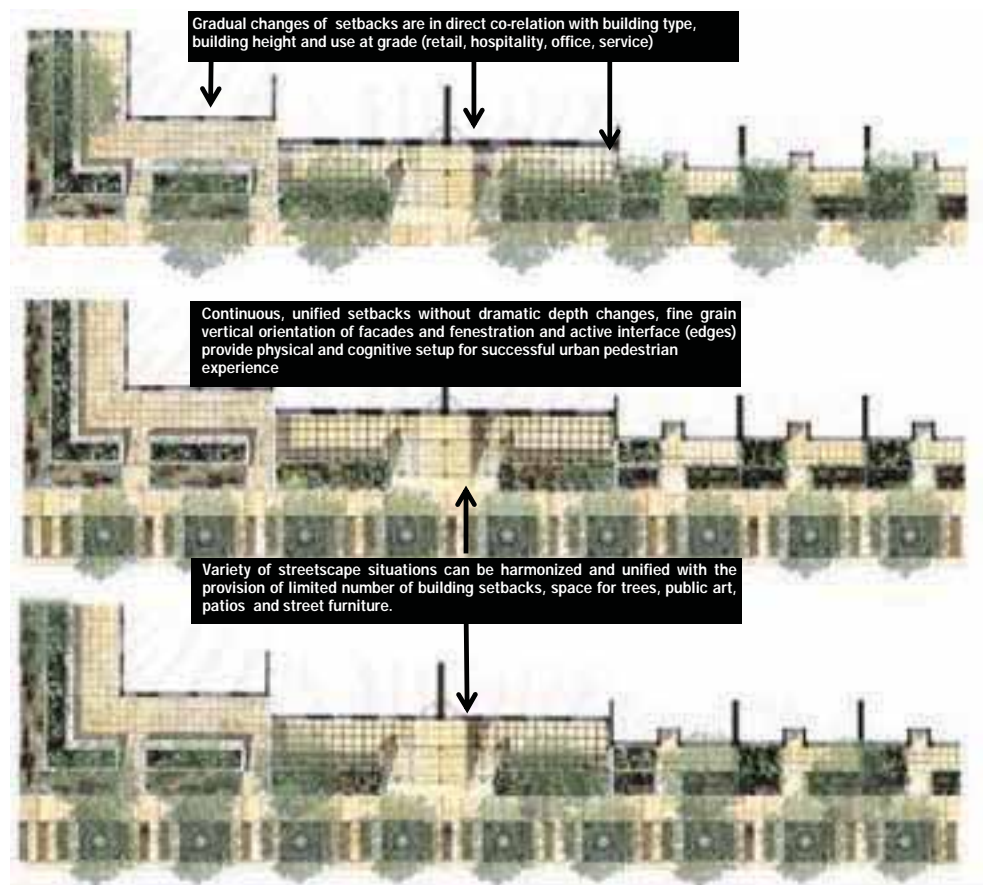
In addition to the Bylaw rules, another level of guideline is needed to preserve the fine-grain, retail character of the Central Precinct.

In general, financial institutions will be strongly discouraged at street level. Where permitted, the maximum storefront width fronting 17 Avenue SW should be no greater than 7.5 metres. This will encourage the inclusion of small “liner shops” to mask the main financial institution floor space, thereby preserving a fine-grain of shops and an active street level.

6.0 Public Realm Guidelines

Much of the character of the avenue derives from the organization and character of the public realm generated from the overall spatial configuration and details of specific elements. Since the existing boundaries of the public realm are inconsistent along the length of the Avenue, the area is

lacking an overall sense of place and identity. These guidelines will help to unify the avenue while building on the existing physical characteristics of the individual precincts.



17th AVENUE PUBLIC REALM — INTRODUCTION

Public realm consists of three main elements: linkages, places and built form, where each has different functions, opportunities and constraints. In high density urban environments, they are found in a variety of relationships and configurations. Careful manipulation of these elements to create a unified urban environment is achieved with urban design guidelines.

Continuity of built form (building or street wall) is essential for the creation of a high quality urban environment, especially for the High Street typology of 17 Avenue SW. Continuous cornice lines, unified setbacks without dramatic depth changes, height and massing of the building base and activities and design of the street wall at street level provide the physical and cognitive environment for a successful public realm.

Treatment of the pedestrian realm /built form interface is related to land use, building typology, building height, activities at-grade, and location of access points. Conflicting situations can be harmonized and unified with the provision of space for trees, public art, attractive storefronts and street lights.

6.1 Pedestrian Realm Guidelines

The existing built condition with respect to curb location, property lines, sidewalk dimensions and building setbacks is inconsistent along the length of the study area. Property lines are typically located 3.0 metres from the back of the curb creating the predominant sidewalk condition. In some cases, commercial buildings are built right to the edge of their property lines in order to create a strong built retail edge, and in other instances, they are set back from this line. Other buildings have been constructed 5.0 metres back from their property line in order to allow for the 5.182 metres bylawed setback requirement in place when 17 Avenue SW was conceived to become a major vehicular thoroughfare. Lastly, there are some buildings whose property lines are located as far back as 8.0 to 10.0 metres from the curb.

In order to unify the pedestrian realm of the Avenue over the long term and to ensure a comfortable width to allow safe pedestrian circulation and a full range of urban “street life” activities, the following dimensions shall be achieved with new development or redevelopment:

East Precinct

North Side:

A minimum building setback of 9.0 metres and patio setback of 4.5 metres from the back of curb.

This wider pedestrian realm is encouraged on the north side for the following reasons:

- it is achievable given that all sites can and likely will be redeveloped over time;
- it allows for greatest sun exposure; and
- it helps offset the higher densities and taller buildings allowed in this location.

South Side:

A minimum building or patio setback of 4.5 metres from the back of curb.

Central and West Precincts

Both Sides:

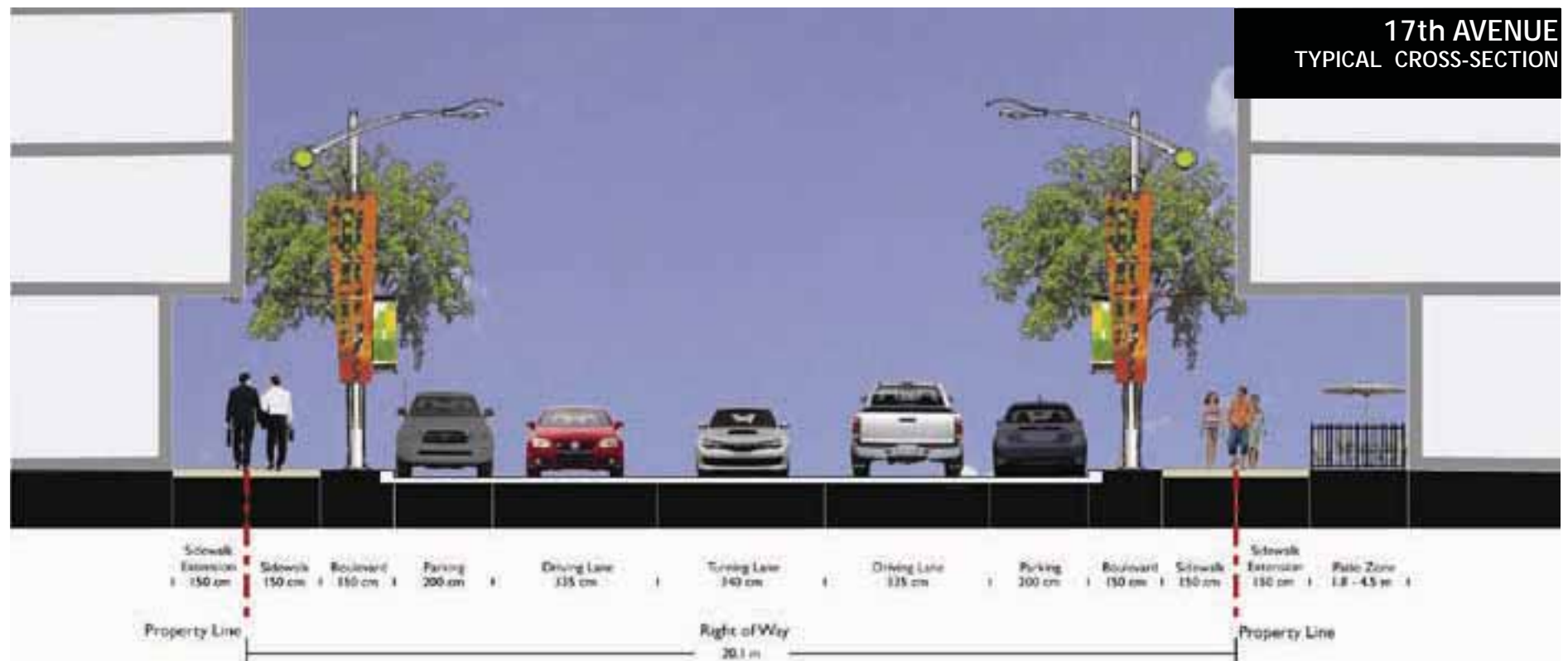
A minimum building or patio setback of 4.5 metres from the back of curb.

6.1.1 “Build to” Incentive

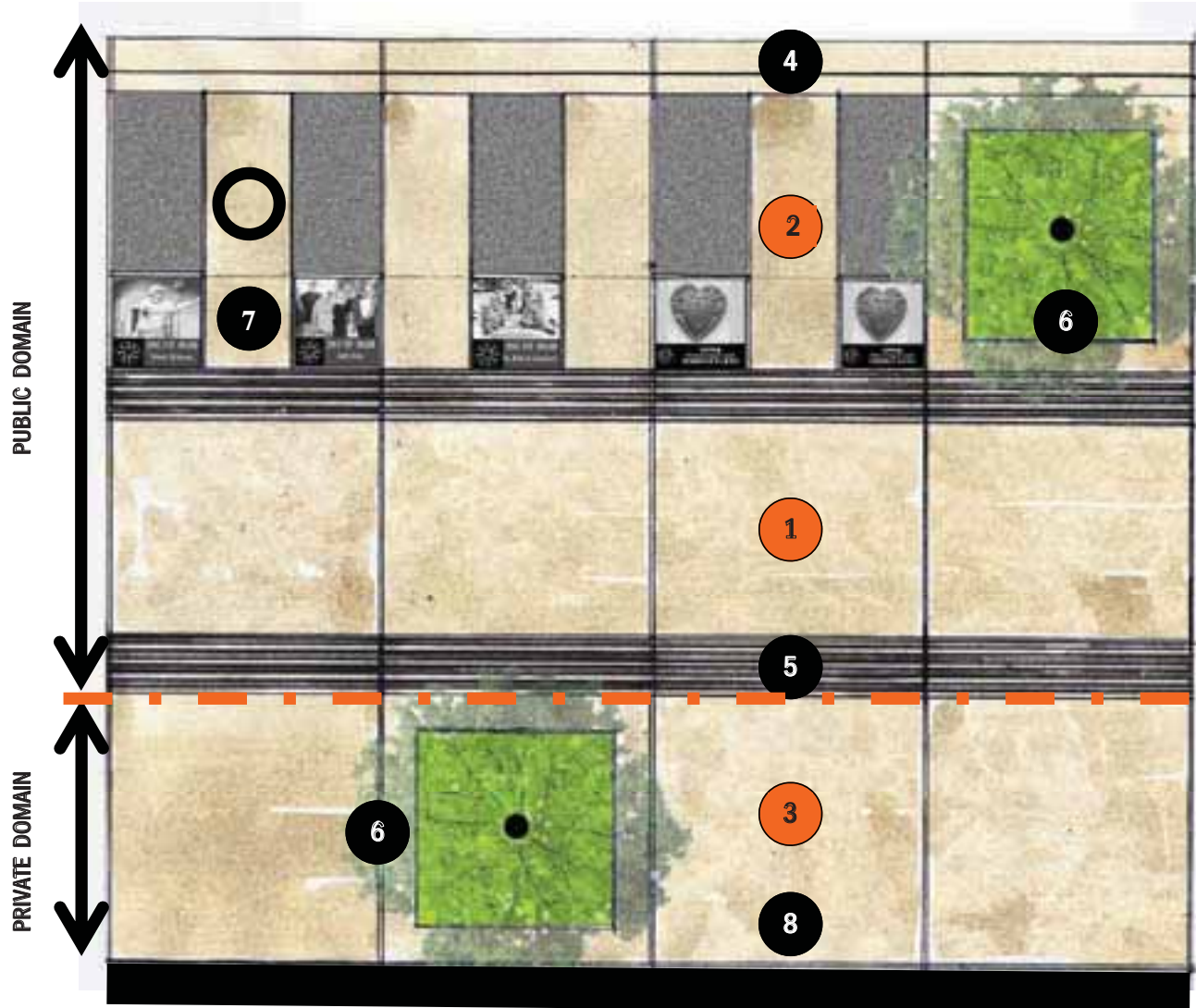
Over time, existing development that has been set back from the street due to the previous bylawed setback is encouraged to “build to” these dimensions. As an incentive, new floor area created for this reason may be exempt from any parking requirement.

6.1.2 Pedestrian Realm Scenarios

The following pages present a variety of scenarios that illustrate in greater detail how these guidelines will be implemented depending on the site specific context. Each drawing also shows the maximum building setbacks for each scenario.



PEDESTRIAN REALM - ZONES



LEGEND

- 1** SIDEWALK ZONE
(unobstructed path)
 - 2** BOULEVARD ZONE
(urban forestry, urban furniture, streetlights, Public transit stops, signage, public art)
 - 3** INTERFACE ZONE
(hardscape / landscape treatment, patios, Seasonal retail activities, temporary signage)
 - 4** CURB
 - 5** URBAN BRAILLE TACTILE GUIDE
 - 6** TREES
 - 7** PUBLIC ART ELEMENTS
 - 8** BUILDING WALL
(retail / office / residential / public art interface)
- — — — —
PROPERTY LINE

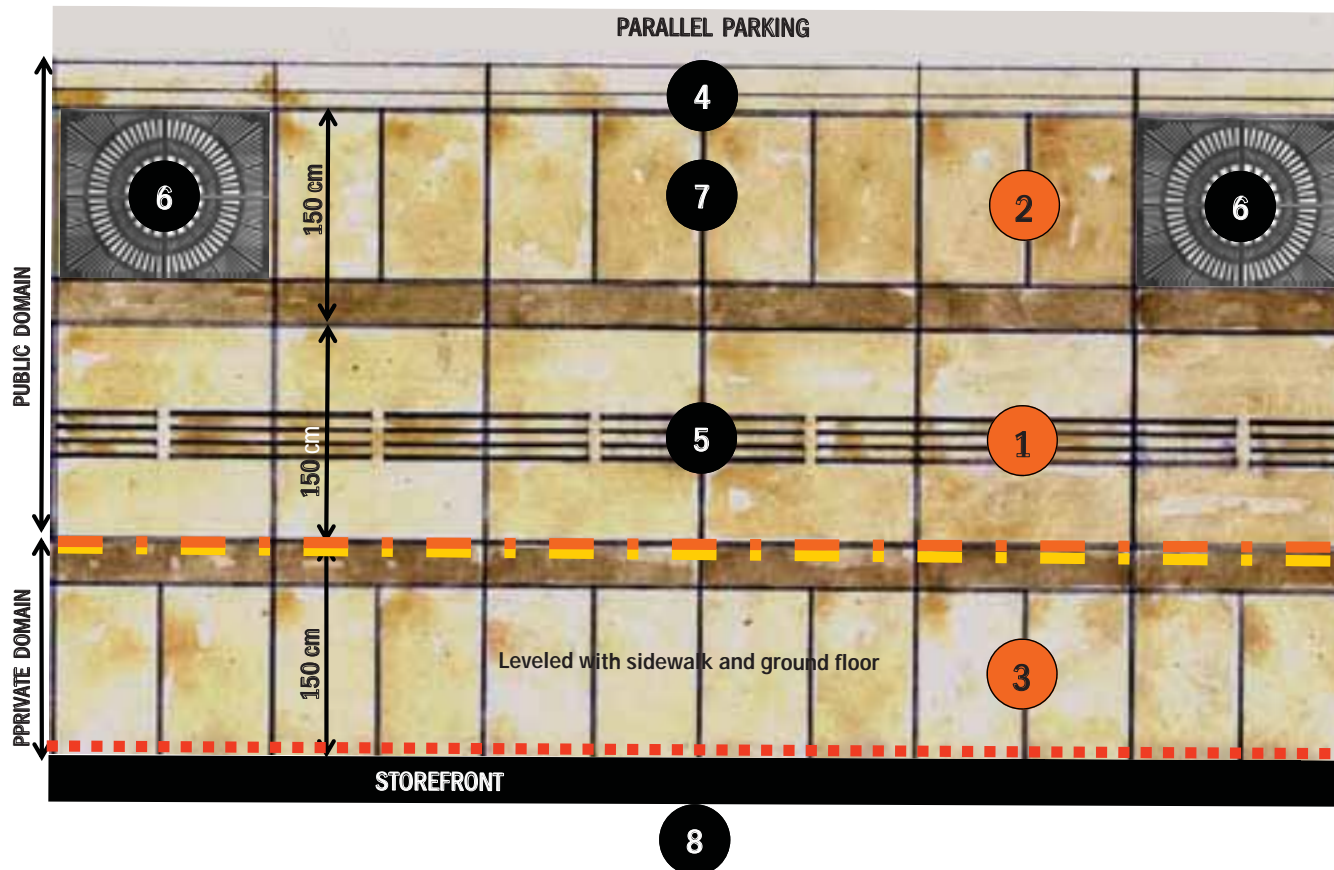


17th AVENUE CALGARY URBAN BRAILLE SYSTEM

LEGEND

- 1 SIDEWALK**
- 2 CROSSWALKS**
- 3 VEHICULAR REALM (Traffic lanes)**
- 4 INTERSECTION**
- 5 BUILDING WALL**
(retail / office / residential / public art interface)
- 1 SIDEWALK DIRECTION TILE**
30 cm Sidewalk Direction Tile—basic prefabricated element, gives sense of direction on unobstructed path
- 2 INTERSECTION NODE TILE**
Basic prefabricated element-Informs about possible direction change
- 3 EDGE WARNING TILE**
(curb ramp)-basic prefabricated element that warns user about change from pedestrian to vehicular realm
- 4 CROSSWALK DIRECTION TILE**
basic prefabricated element that guides user about change from pedestrian to vehicular realm





17th AVENUE URBAN EDGE STANDARD, TYPE 1

LEGEND

- 1** SIDEWALK ZONE
(unobstructed path)
 - 2** BOULEVARD ZONE
(urban trees and furniture, streetlights, Public Transit stops, signage, wayfinding, public art)
 - 3** INTERFACE ZONE
(hardscape / landscape treatment, patios, seasonal retail activities, temporary signage outside of bus shelter zones)
 - 4** CURB
 - 5** URBAN BRAILLE TACTILE GUIDE
 - 6** TREES (public domain)
 - 7** LOCATION FOR PUBLIC ART
 - 8** BUILDING WALL
(retail / office / residential / public art interface)
- — — — —
PROPERTY LINE
- — — — —
UNDERGROUND GARAGE LINE
- — — — —
BUILDING OVERHANG: Max.. 1.5 m
from building storefront face

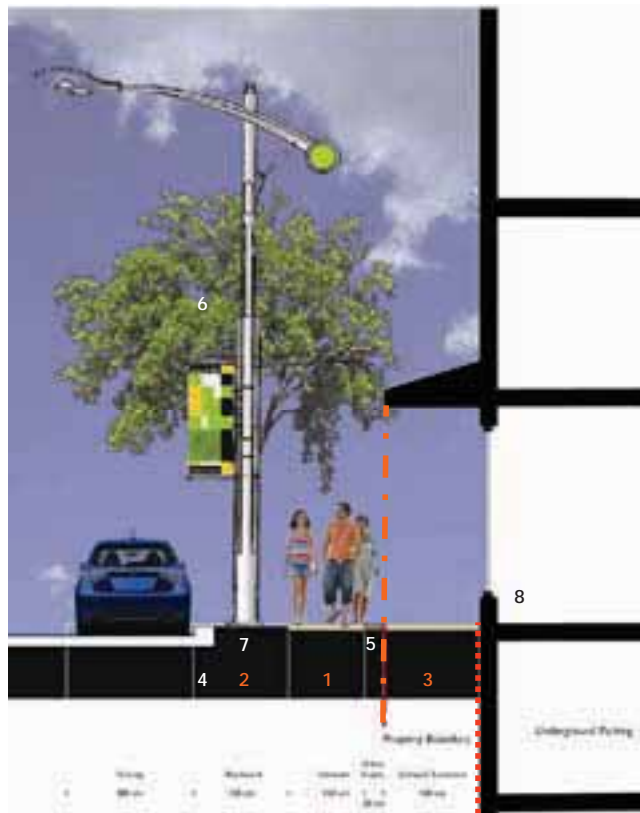
17th AVENUE URBAN EDGE STANDARD, TYPE 1

LEGEND

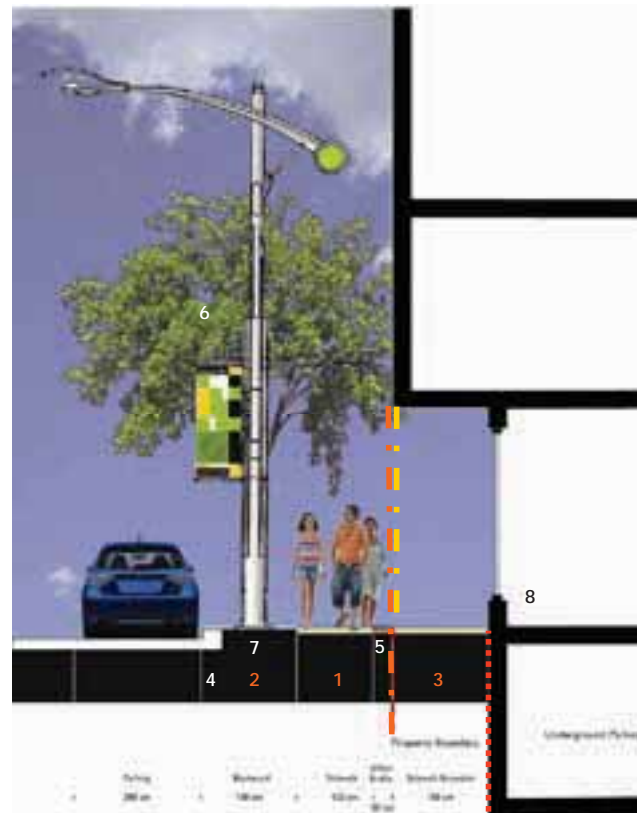
- 1** SIDEWALK ZONE
(unobstructed path)
- 2** BOULEVARD ZONE
(urban trees and furniture, streetlights, Public Transit stops, signage, wayfinding, public art)
- 3** INTERFACE ZONE
(hardscape / landscape treatment, patios, seasonal retail activities, temporary signage outside of bus shelter zones)
- 4** CURB
- 5** URBAN BRAILLE TACTILE GUIDE
- 6** TREES (public domain)
- 7** LOCATION FOR URBAN FURNITURE & PUBLIC ART
- 8** BUILDING WALL
(retail / office / residential / public art interface)

— — — — —
PROPERTY LINE
— — — — —
UNDERGROUND GARAGE LINE

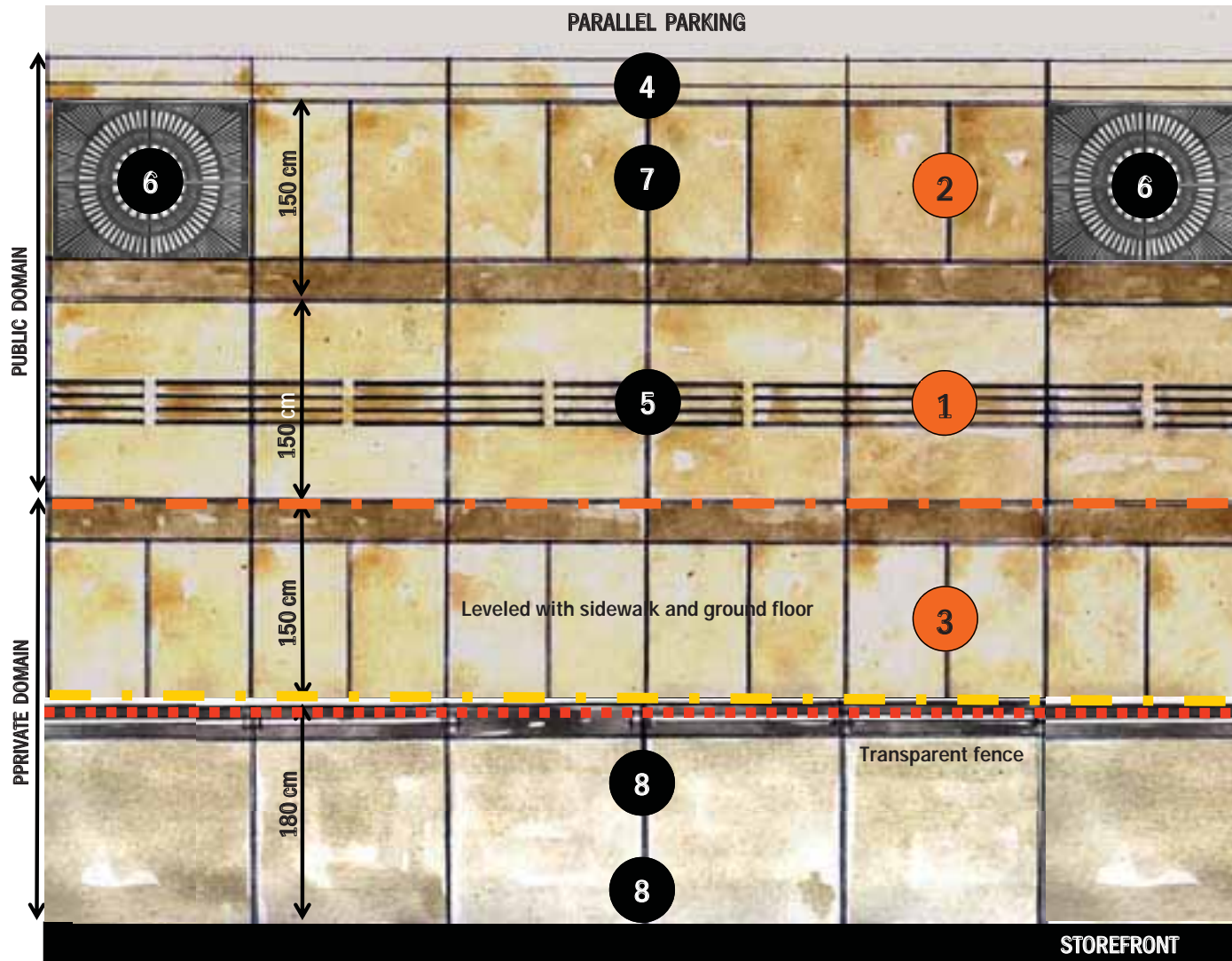
BUILDING OVERHANG: Max.. 1.5 m
from building storefront face



ALTERNATIVE 1



ALTERNATIVE 2



17th AVENUE URBAN EDGE STANDARD, TYPE 2

LEGEND

- 1** SIDEWALK ZONE
(unobstructed path)
- 2** BOULEVARD ZONE
(urban trees and furniture, streetlights, Public Transit stops, signage, wayfinding, public art)
- 3** INTERFACE ZONE
(hardscape / landscape treatment, patios, seasonal retail activities, temporary signage outside of bus shelter zones)
- 4** CURB
- 5** URBAN BRAILLE TACTILE GUIDE
- 6** TREES (public domain)
- 7** LOCATION FOR STREET FURNITURE & PUBLIC ART
- 8** SEASONAL PATIO
- 9** BUILDING WALL
(retail / office / residential / public art)

- PROPERTY LINE
- UNDERGROUND GARAGE LINE
- BUILDING OVERHANG: Max.. 1.8 m from building storefront face

17th AVENUE URBAN EDGE STANDARD, TYPE 2

LEGEND

- 1** SIDEWALK ZONE
(unobstructed path)
- 2** BOULEVARD ZONE
(urban trees and furniture, streetlights, Public Transit stops, signage, wayfinding, public art)
- 3** INTERFACE ZONE
(hardscape treatment, patios, seasonal retail activities, temporary signage outside of bus shelter zones)
- 4** CURB
- 5** URBAN BRAILLE TACTILE GUIDE
- 6** TREES (public domain)
- 7** LOCATION FOR STREET FURNITURE & PUBLIC ART
- 8** LOCATION FOR STREET FURNITURE & PUBLIC ART
- 9** SEASONAL PATIO

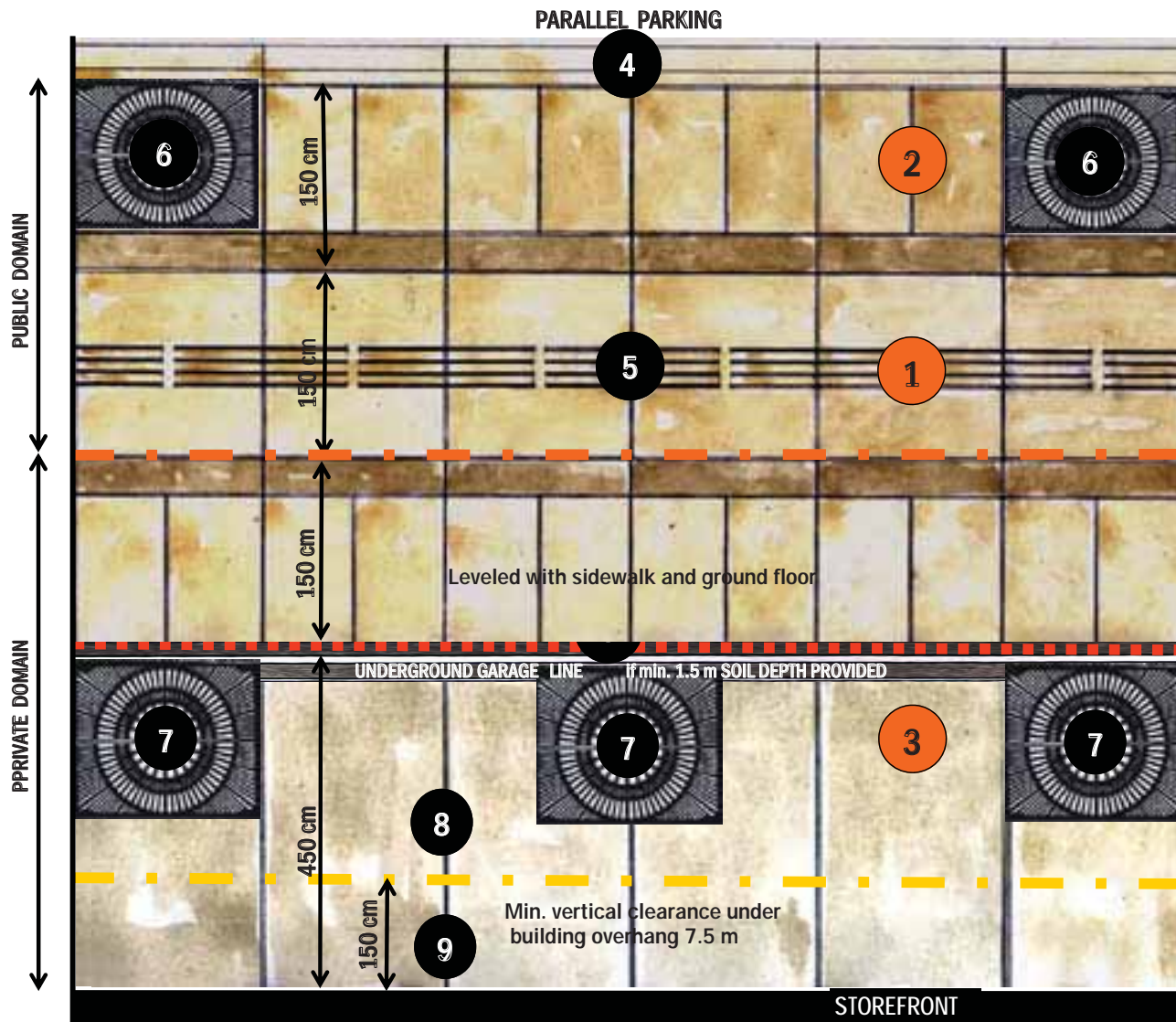
- PROPERTY LINE
- UNDERGROUND GARAGE: Min. setback 0.15
- BUILDING OVERHANG: Max. 1.5 m from building face



ALTERNATIVE 1



ALTERNATIVE 2



17th AVENUE URBAN EDGE STANDARD, TYPE 3

LEGEND

- 1** SIDEWALK ZONE
(unobstructed path)
 - 2** BOULEVARD ZONE
(urban trees and furniture, streetlights, Public Transit stops, signage, wayfinding, public art)
 - 3** INTERFACE ZONE
(hardscape / landscape treatment, patios, seasonal retail activities, temporary signage outside of bus shelter zones)
 - 4** CURB
 - 5** URBAN BRAILLE TACTILE GUIDE
 - 6** TREES (public domain)
 - 7** TREES (private domain)
 - 8** RESTAURANT PATIO
(Large)
 - 9** BUILDING WALL
(retail / office / residential / public art interface)
- — — — —
PROPERTY LINE
- — — — —
UNDERGROUND GARAGE: Min. setback 0.15 m
- — — — —
BUILDING OVERHANG: Max. 1.5 m from building face



17th AVENUE URBAN EDGE STANDARD, TYPE 3

LEGEND

- 1** SIDEWALK ZONE
(unobstructed path)
- 2** BOULEVARD ZONE
(urban trees and furniture, streetlights, Public Transit stops, signage, wayfinding, public art)
- 3** INTERFACE ZONE
(hardscape / landscape treatment, patios, seasonal retail activities, temporary signage outside of bus shelter zones)
- 4** CURB
- 5** URBAN BRAILLE TACTILE GUIDE
- 6** TREES (public domain)
- 7** TREES (private domain)
- 8** LOCATION FOR STREET FURNITURE & PUBLIC ART
- 9** SEASONAL PATIO

PROPERTY LINE
 UNDERGROUND GARAGE: Min. setback
 BUILDING OVERHANG: Max. 1.5 m from building face



17th AVENUE URBAN EDGE STANDARD, TYPE 4

LEGEND

- 1** CURB
- 2** URBAN FURNITURE & PUBLIC ART LOCATION
- 3** TREES (Public domain)
- 4** URBAN BRAILLE TACTILE GUIDE
- 5** TREES (private domain)
- 6** TREE PLANTER
Max. 120 cm above the grade
(columnar trees only)
- 7** UPPER LEVEL: PATIO / RETAIL
LOWER LEVEL: RETAIL+LIVE / WORK
- 8** BUILDING WALL
(retail / office / residential / public art interface)



BUILDING OVERHANG: Max.. 4.5 m
from building storefront face

17th AVENUE URBAN EDGE STANDARD, TYPE 4



LEGEND

- | | |
|--|--|
| 1 CURB | 5 TREES (private domain) |
| 2 URBAN FURNITURE & PUBLIC ART LOCATION | 6 TREE PLANTER
Max. 120 cm above the grade (columnar trees only) |
| 3 TREES (Public domain) | 7 UPPER LEVEL: PATIO / RETAIL
LOWER LEVEL: RETAIL+LIVE / WORK |
| 4 URBAN BRAILLE TACTILE GUIDE | 8 BUILDING WALL
(retail / office / residential / public art interface) |





17th AVENUE URBAN EDGE STANDARD, TYPE 5

LEGEND

- 1 CURB
- 2 URBAN FURNITURE & PUBLIC ART LOCATION
- 3 TREES (Public domain)
- 4 URBAN BRAILLE TACTILE GUIDE
- 5 TREES (Private domain)
- 6 PLANTER WITH COLUMNAR OR OTHER DECORATIVE TREES
- 7 COLONNADE: EXTENDED SIDEWALK + PATIO+RETAIL+LIVE/WORK COURTYARD
- 8 BUILDING WALL (retail / office / residential / public art interface)

- PROPERTY LINE
- UNDERGROUND GARAGE LINE
- BUILDING OVERHANG: Max.. 4.5 m from building storefront face

Urban Edge Standards by Precinct

Determining which urban edge standard is used for a particular site will depend on where the site is located. This table illustrates the preferred urban edge standard by precinct.

	PRECINCTS		
	East	Central	West
TYPE 1	●	●	●
TYPE 2	--	●	●
TYPE 3	● *	--	--
TYPE 4	●	● *	● *
TYPE 5	●	--	--

* Preferred on North side

6.2 Built Form Guidelines

The term “context” involves the consideration of buildings beyond a particular site’s immediate neighbours. An evaluation of existing sites along 17 Avenue should always include massing, scale, height, proportions of fenestration, nature and quality of materials used, textures and colours.

One of the most important elements in urban design is rhythm – an orderly alternation of primary, secondary and tertiary elements of a building, block or precinct. When moving along the street as a pedestrian, one subconsciously experiences a rhythm of building mass to building openings. There is also a rhythmic relationship between tertiary elements such

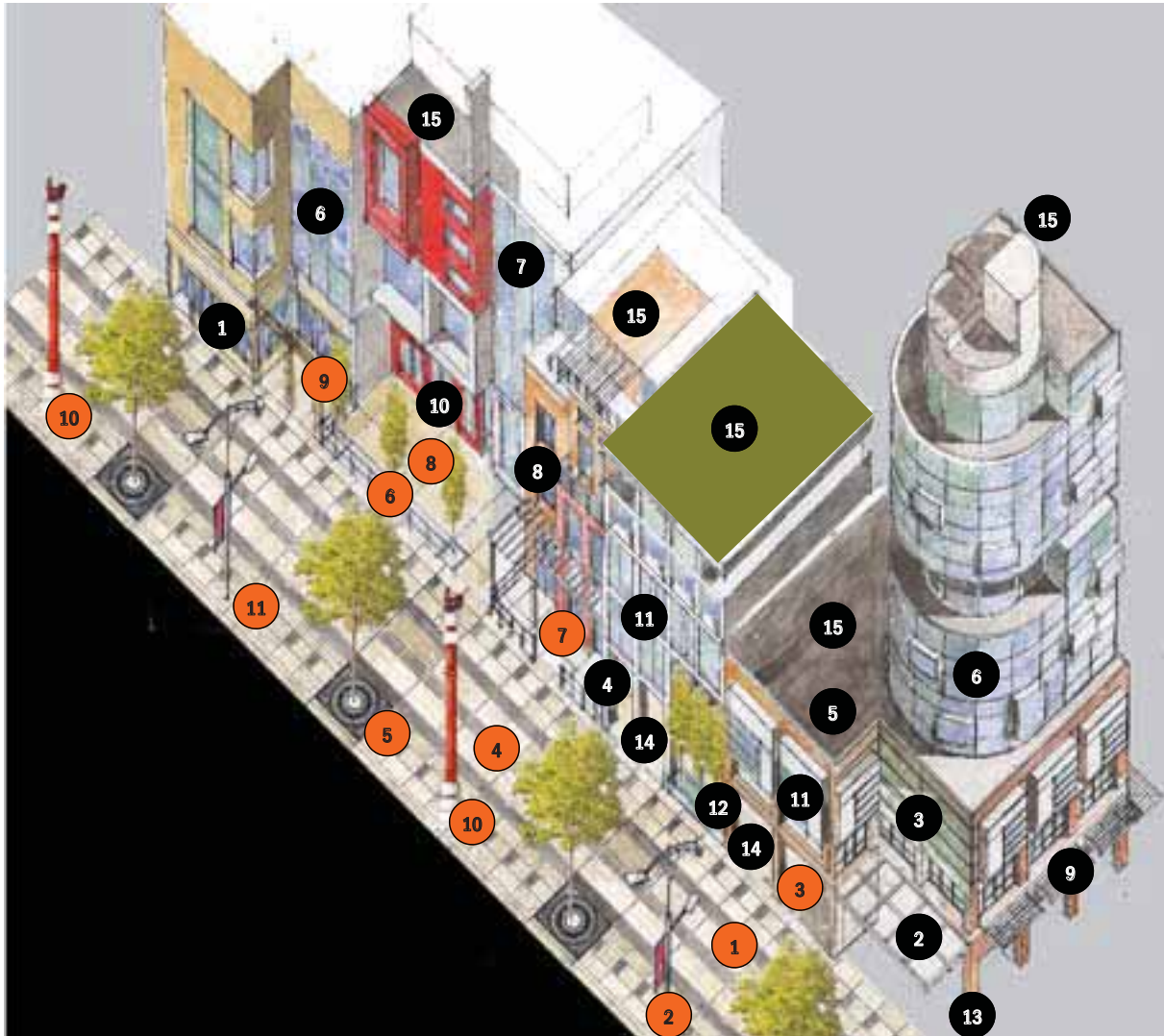
as entrances, storefronts, signage, etc. All these elements directly influence the perception of “human scale” and our experience of the urban environment. A building or block along 17 Avenue SW may continue a certain streetscape rhythm through various aspects of its structure or shape and placement of its openings. New infill development may establish new points of symmetry and balance and may strengthen existing patterns or stimulate the establishment of new built form patterns. The following guidelines are intended to facilitate change, acknowledge the best existing patterns and suggest the best solutions for future low, medium and high density infill projects.

17th AVENUE BUILT FORM GUIDELINES

INFILL



- 1 Building edges that are oriented toward a public right-of-way should be lined with uses that create activity and provide natural surveillance.
- 2 Canopies or other architectural features that are integrated with the overall building design should be provided to protect pedestrians from inclement weather.
- 3 The base of the building should be designed to create a human scaled street wall and establish a strong visual rhythm.
- 4 New infill development shall avoid the creation of blank building facades. Facades shall be pedestrian scaled, create visual interest and use fully transparent glass.
- 5 There should be strongly defined transition between the base and the body of a building through the use of setbacks, materials or other applicable architectural treatments such as cornices, canopies, or trellises.
- 6 Body materials may differ from base materials, but compatibility and transition between materials should be considered and the rhythm of the lower floors should be respected. Building elements in this zone may have a "lighter" appearance with more glazing than used at the base.
- 7 Special consideration should be given to the excessive use of dark coloured reflective glass and textures in order to prevent negative light reflection impacts on surrounding properties.
- 8 Fully or partially recessed balconies, regardless of size, are preferred over "tacked on" balconies, especially on higher floors.
- 9 Weather protection features that are integral to the architectural design of the building are encouraged along street with commercial activities where retail and office spaces are located on lower portions of the building, or where the public might congregate, such as transit stops.
- 10 Street front elevations shall be highly transparent and allow for unobstructed pedestrian views directly into the business for minimum of two-thirds of the façade length.
- 11 Facades shall be highly articulated and incorporate vertically oriented fenestration and other applicable architectural elements that suggest a rhythm of narrower business frontages. As a guideline, a business frontage width of 7.5 m is considered to be a useful benchmark.
- 12 New developments are encouraged to consider provision of below-grade uses that can be accessed directly from the sidewalk.
- 13 All rooftops, including podiums of taller buildings are encouraged to incorporate landscape amenities or green roofs in order to achieve living, aesthetic and environmental benefits.
- 14 The base of infill buildings should be capped with strong cornice lines or similar contemporary architectural features that maintain the street continuity of existing buildings.

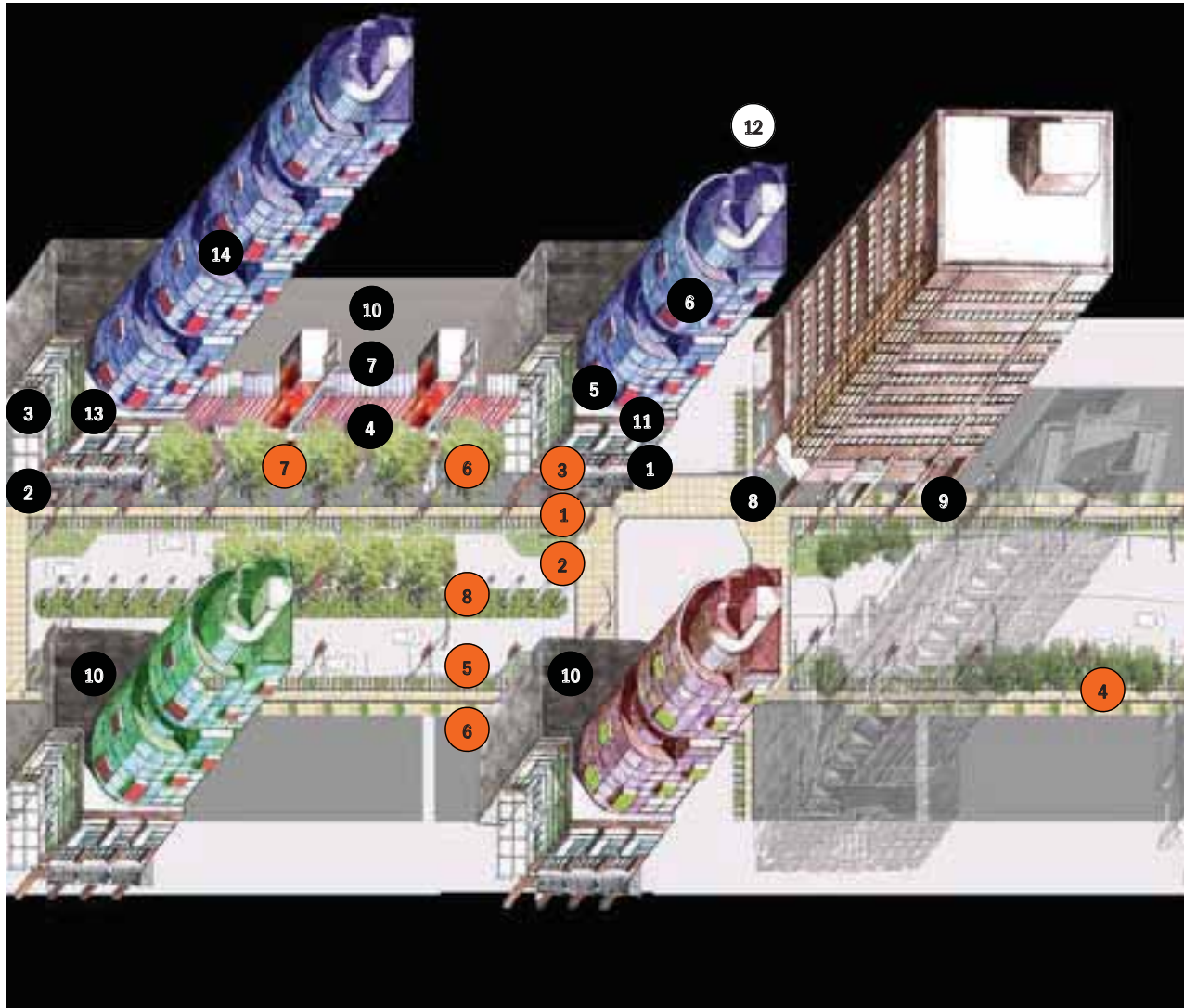


17th AVENUE STREETSCAPE PRINCIPLES

CENTRAL & WESTERN PRECINCTS

- 1 SIDEWALK ZONE (unobstructed path)
- 2 BOULEVARD ZONE
(urban forestry, urban furniture, streetlights,
Public transit stops, signage, public art)
- 3 INTERFACE ZONE (hardscape / landscape
treatment, patios, Seasonal retail activities, tem-
porary signage)
- 4 CALGARY URBAN BRAILLE
- 5 TREES (Public domain)
- 6 TREES (Private domain)
- 7 SMALL PATIO
- 8 LARGE PATIO
- 9 PORTICO / GATEWAY / ARTISTIC
TRANSPARENT SCREEN
- 10 PUBLIC ART
- 11 URBAN FURNITURE

- 1 Building edges that are oriented toward a public right-of-way should be lined with uses that create activity and provide natural surveillance.
- 2 Canopies or other architectural features that are integrated with the overall building design should be provided to protect pedestrians from inclement weather.
- 3 The base of the building should be designed to create a human scaled street wall and establish a strong visual rhythm.
- 4 New infill development shall avoid the creation of blank building facades. Facades shall be pedestrian scaled, create visual interest and use fully transparent glass.
- 5 There should be strongly defined transition between the base and the body of a building through the use of setbacks, materials or other applicable architectural treatments such as cornices, canopies, or trellises.
- 6 Body materials may differ from base materials, but compatibility and transition between materials should be considered and the rhythm of the lower floors should be respected. Building elements in this zone may have a "lighter" appearance with more glazing than used at the base.
- 7 Special consideration should be given to the excessive use of dark coloured reflective glass and textures in order to prevent negative light reflection impacts on surrounding properties.
- 8 Fully or partially recessed balconies, regardless of size, are preferred over "tacked on" balconies, especially on higher floors.
- 9 Weather protection features that are integral to the architectural design of the building are encouraged along street with commercial activities where retail and office spaces are located on lower portions of the building, or where the public might congregate, such as transit stops.
- 10 Street front elevations shall be highly transparent and allow for unobstructed pedestrian views directly into the business for minimum of two-thirds of the façade length.
- 11 Facades shall be highly articulated and incorporate architectural elements that suggest a rhythm of narrower business frontages. As a guideline, a business frontage width of 7.5 m is considered to be a useful benchmark.
- 12 New developments are encouraged to consider provision of below-grade uses that can be accessed directly from the sidewalk.
- 13 Corner treatments may include full built-to-line corners, chamfered, semi-circled or other types of geometric treatments. Corner treatments should include single or multiple entrances, full visual transparency at grade and protection from weather elements in form of colonnades or architecturally integrated canopies.
- 14 Use colonnades for weather protection, and where opportunity exists to extend the public realm by providing a wider sidewalk area. Colonnades may also be used for restaurant patios if sufficient size. Colonnades shall not extend into the public right-of-way.
- 15 All rooftops, including podiums of taller buildings are encouraged to incorporate landscape amenities or green roofs in order to achieve living, aesthetic and environmental benefits.



17th AVENUE STREETSCAPE PRINCIPLES

EASTERN PRECINCT

- 1** SIDEWALK ZONE (unobstructed path)
- 2** BOULEVARD ZONE
(urban forestry, urban furniture, streetlights,
Public transit stops, signage, public art)
- 3** INTERFACE ZONE (hardscape / landscape
treatment, patios, Seasonal retail activities, tem-
porary signage)
- 4** CALGARY URBAN BRAILLE
- 5** TREES (Public domain)
- 6** TREES (Private domain)
- 7** LARGE PATIO
- 8** PUBLIC ART

- 1 Building edges that are oriented toward a public right-of-way should be lined with uses that create activity and provide natural surveillance.
- 2 Canopies or other architectural features that are integrated with the overall building design should be provided to protect pedestrians from inclement weather.
- 3 The base of the building should be designed to create a human scaled street wall and establish a strong visual rhythm.
- 4 New infill development shall avoid the creation of blank building facades. Facades shall be pedestrian scaled, create visual interest and use fully transparent glass.
- 5 There should be strongly defined transition between the base and the body of a building through the use of setbacks, materials or other applicable architectural treatments such as cornices, canopies, or trellises.
- 6 Body materials may differ from base materials, but compatibility and transition between materials should be considered and the rhythm of the lower floors should be respected. Building elements in this zone may have a “lighter” appearance with more glazing than used at the base.
- 7 For comprehensive block re-developments facades shall be highly articulated and incorporate architectural elements that suggest a rhythm of narrower business frontages. As a guideline, a business frontage width of 7.5 m is considered to be a useful benchmark.
- 8 Corner treatments may include full built-to-line corners, chamfered, semi-circled or other types of geometric treatments. Corner treatments should include single or multiple entrances, full visual transparency at grade and protection from weather elements in form of colonnades or architecturally integrated canopies.
- 9 Use colonnades for weather protection, and where opportunity exists to extend the public realm by providing a wider sidewalk area. Colonnades may also be used for restaurant patios if sufficient size. Colonnades shall not extend into the public right-of-way.
- 10 All rooftops, including podiums of taller buildings are encouraged to incorporate landscape amenities or green roofs in order to achieve living, aesthetic and environmental benefits.
- 11 The base of infill buildings should be capped with strong cornice lines or similar contemporary architectural features that maintain the street continuity of existing buildings.
- 12 It is encouraged that tower tops contribute to the skyline profile by having reasonably identifiable or iconic architectural design that skillfully incorporates within the tower top all elevator cores and mechanical rooms.
- 13 Building bases are encouraged to use masonry or other durable materials and other architectural details that establish a strong vertical rhythm with human scaled elements.
- 14 Body materials may differ from the base materials, but compatibility and transition between materials should be considered and the rhythm of the lower floors should be respected. Building in this zone may have a “lighter” appearance with more glazing than used at the base.



This image of a High Street from the Centre City Plan exemplifies the desired character for 17 Avenue SW as it develops over time.

This image represents a typical section of the Central Precinct with the incorporation of street trees consistent along the section.



7.0 Case Studies

In order to significantly enhance the quality and character of 17 Avenue SW, four major opportunities have been identified to create new or improve on existing nodes. These nodes help to “anchor” the avenue and create a dynamic and interesting experience as one moves along the corridor. Each precinct is represented in the case studies.

The role of these case studies is to inspire both private and public redevelopment without prescribing or committing to specific detailed solutions. They can serve as a guide for City initiated projects and a suggestion for private redevelopment opportunities.

The analysis, design and illustration of these case studies was undertaken by Sturgess Architecture as part of their contribution to the 17 Avenue SW Urban Design Strategy.

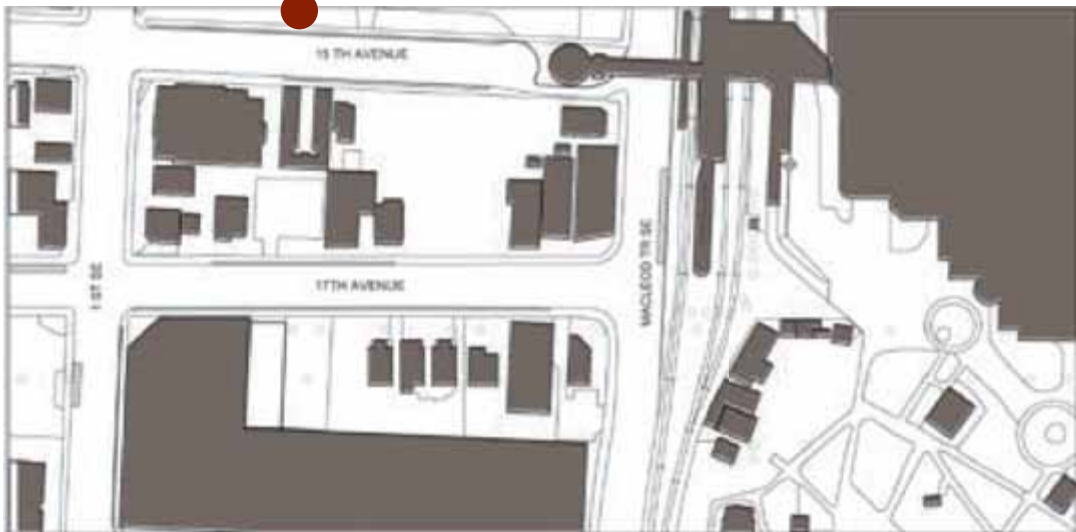
7.1 East Precinct Context:

Case Study 1: Stampede Entrance

Context:

- There is no current visual terminus of 17 Avenue SE at the east end
- Macleod Trail connection to Stampede Park should be exploited
- Public realm should be expanded to accept greater at-grade traffic related to Stampede and other events.
- Greater residential density is expected in the first block from the east.

Existing Plan

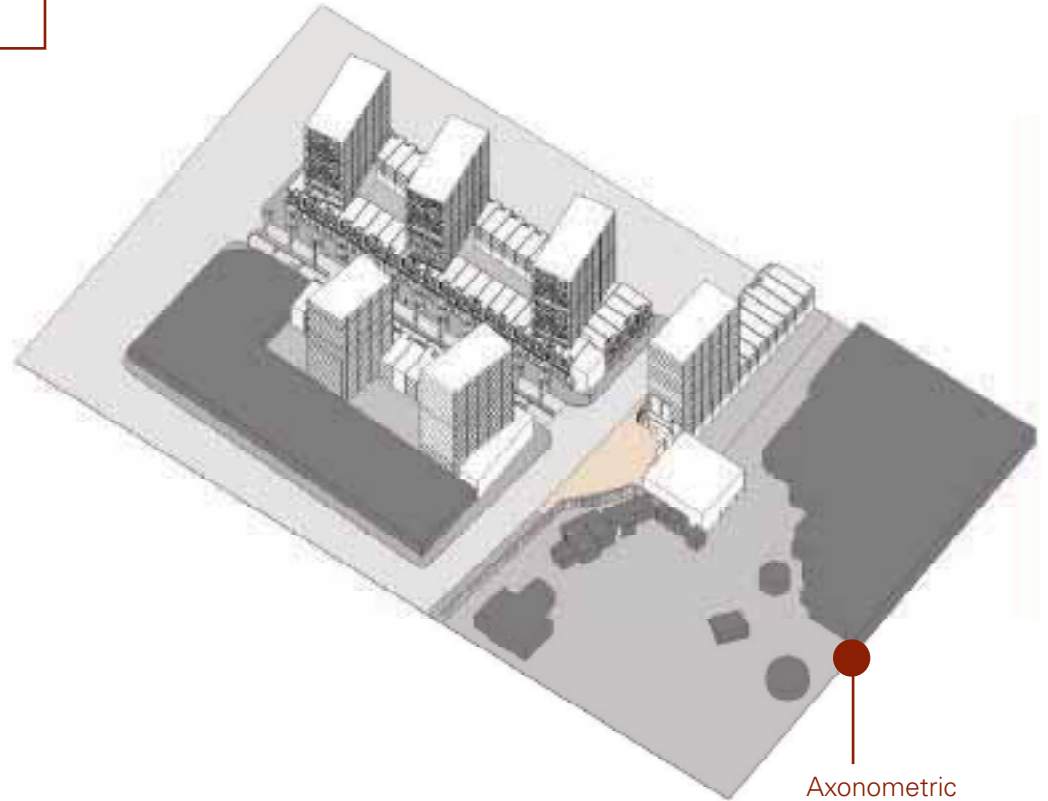


Design Principles:

- Expand sidewalk width/ pedestrian zone
- Create a connection through the Stampede grounds, over the LRT tracks for both vehicular and pedestrian/ bicycle traffic
- Consistent with the *Centre City Plan*, the possibility of burying the LRT adjacent to Stampede should be investigated. This would significantly increase the presence and ceremony of Stampede Park at 17 Avenue SE.
- Provide an increased public realm possibly through the incorporation of a plaza and entrance features on the Stampede grounds
- Utilize the greater building setbacks to create a row of trees on private lands as an alternate to a central, planted median.



Proposed Plan



Axonometric



Perspective

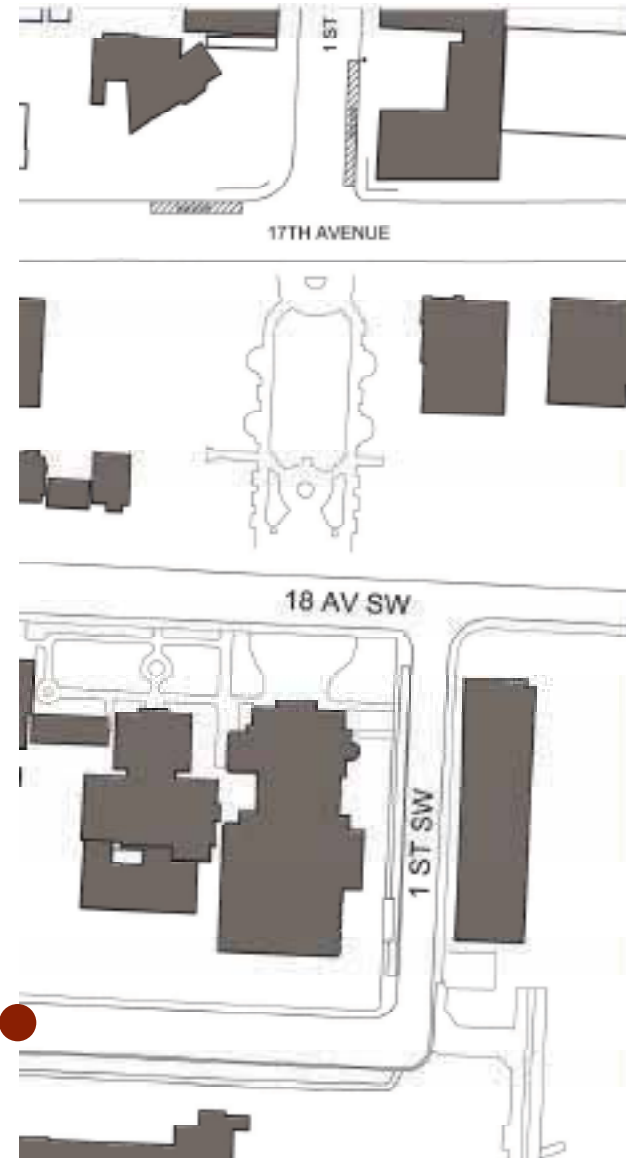
Case Study 2: Rouleauville Park

Context:

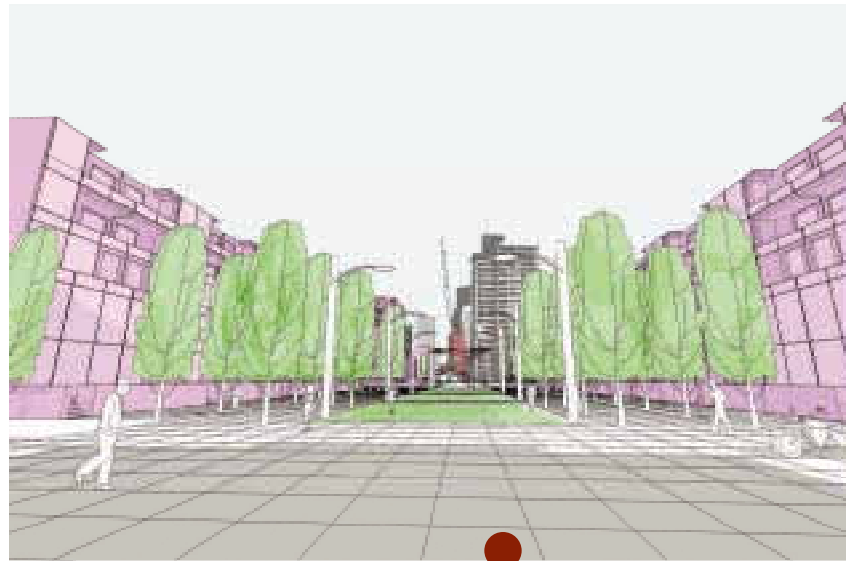
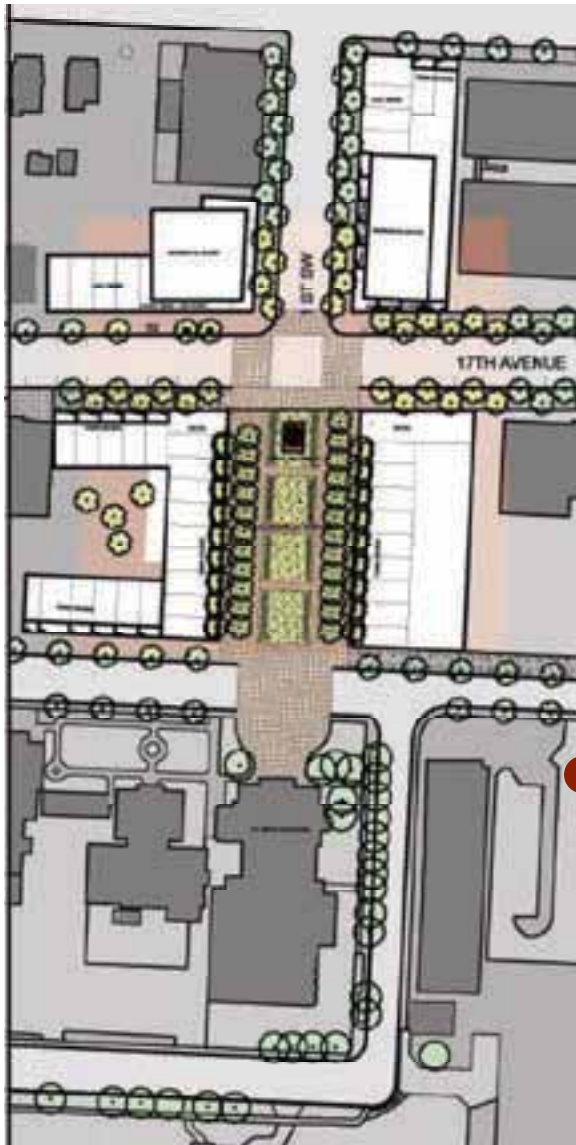
- Historic St. Mary's Cathedral and Rouleauville Park
- Existing context includes church buildings and schools, mixed single family and multi-family housing and retail along 17 Avenue SW
- 1 ST. SW has a mix of retail and high-rise residential towers

Design Principles

- Enhance the pedestrian connection through Rouleauville Square to the Cathedral and the Alberta Ballet Building to the Elbow River
- Frame Rouleauville Square with low-rise residential buildings
- Extend plaza treatment to the Cathedral
- Enhance view of St. Mary's Cathedral from 17 Avenue SW by removing existing signage adjacent to 17 Avenue SW
- Live-work/residential extends along 1 St SW
- Double row of trees is provided along 17 Avenue SW

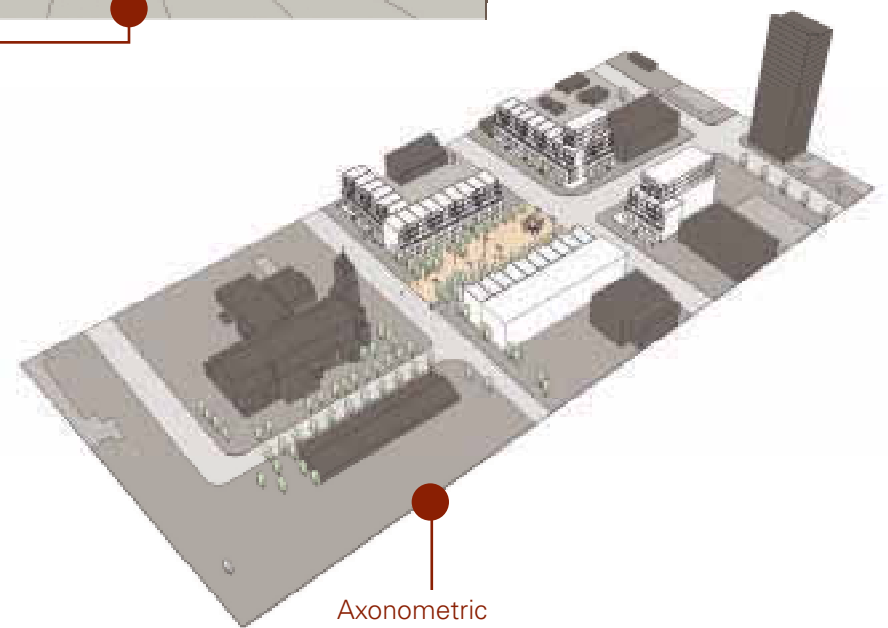


Existing Plan



Perspective

Proposed Plan



Axonometric

7.2 Central Precinct

Case Study 3: Tomkins Park

Context:

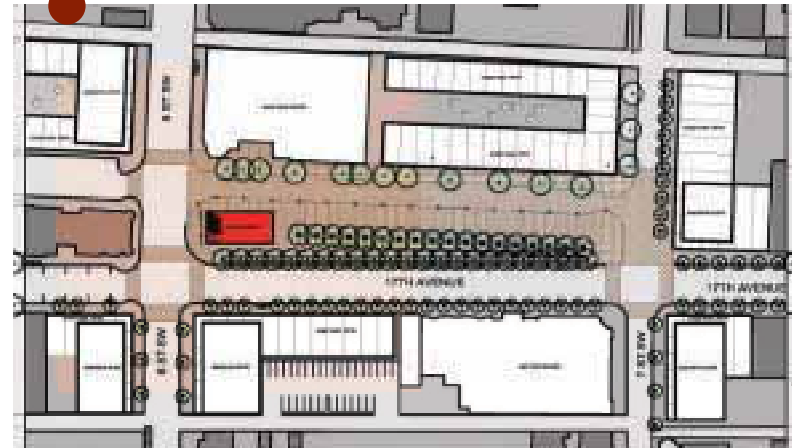
- Historic Devenish Building faces directly onto Tomkins Park
- Potential for new retail/residential mix
- Intensive office, retail and residential mixed use area
- Major intersection of 8th St S.W. and 17 Ave S.W.

Design Principles:

- Extend plaza treatment and design to incorporate adjacent streets
- Provide a double row of street trees at 17 Avenue SW edge and prune trees with a higher canopy
- At-grade parking is still permitted on the plaza delineated by bollards
- At-grade parking from the lane permitted where warranted
- Consider a new pedestrian bulb adjacent to the Devenish building to increase pedestrian realm
- Signify confluence of 8th St S.W. and 17 Ave S.W with a significant piece of public art
- Incorporate significant decorative lighting

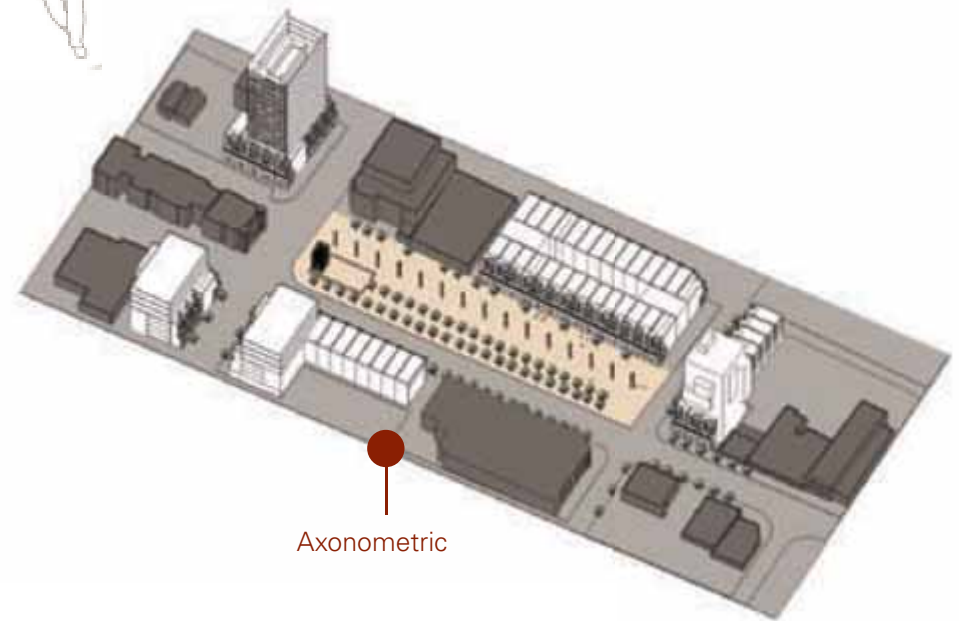
Existing Plan

Proposed Plan





Perspective



Axonometric

7.3 West Precinct

Case Study 4: 11 Street S.W. Intersection

Context:

- Existing marginalized retail strip parking, large out of scale signage
- Connections to 11th Street at 17 Avenue SW are disconnected and need to be enhanced

Design Principles:

- Provide a new park on the corner of 11th Street and 17 Avenue SW by relocating the park on 16th Avenue to 17 Avenue SW
- Provide retail/residential edge to the new park
- Align residential buildings to line up axially with 11th Street
- Complete retail multi-family housing edge where appropriate
- Establish a non-vehicular axial alignment north of 11th St S.W.

Existing Plan

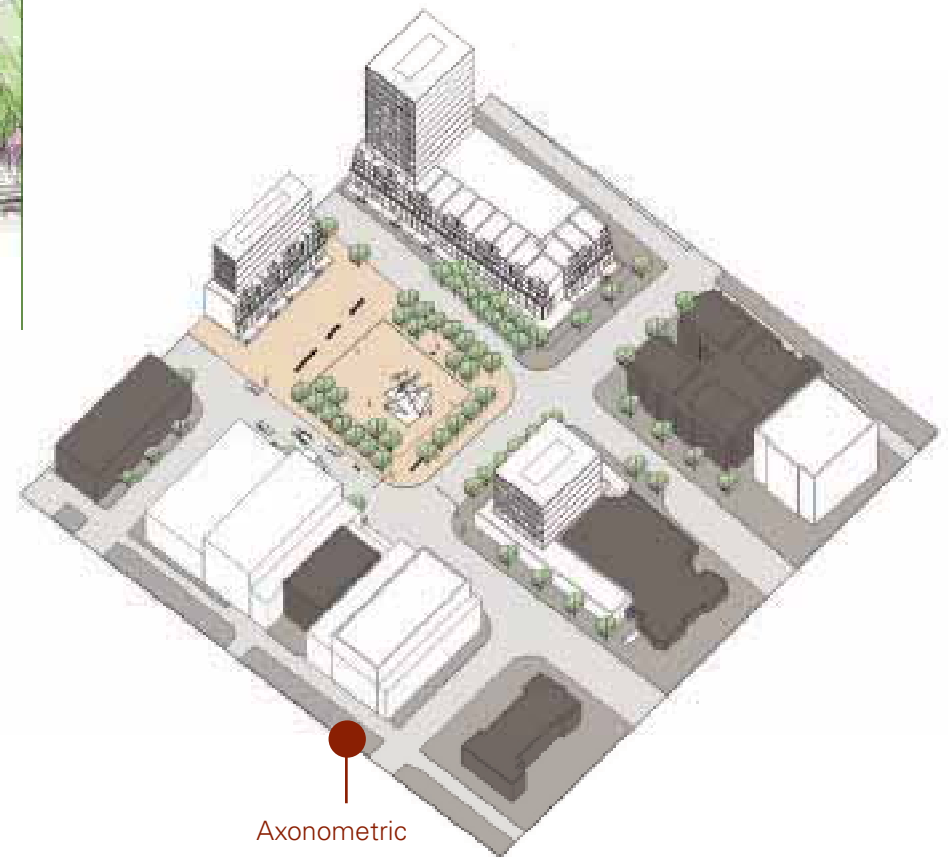


Proposed Plan





Perspective



Axonometric

8.0 Implementation

The 17 Avenue SW Urban Design Strategy will be implemented through the following methods:

1. The development application review process.
2. City-initiated public improvement projects, coordinated through:
 - The Business Planning and Budget Coordination Process;
 - Land Use Planning and Policy Work Program; and
 - Centre City Implementation Work Programs.
3. Public improvement projects may be funded through the Beltline Community Investment Fund and other methods to be determined on a case by case or site by site basis.
4. Other possible methods and processes to be determined.



